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F. No. 23(29-I)/CPM/F 1/PWD/2016/616

Date: 7-3-16

### Inspection Note

**Sub: - Inspection Note for the work of “Comprehensive Development of Corridor (Outer Ring Road) between Vikaspuri to Meera Bagh.”**

Inspected the site of Vikaspuri to Meerabagh Project on 04.03.2016 along with the Superintending Engineer, F-12 and Project Manager of M/s. Simplex.

First, went to the Najafgarh Drain and reviewed the progress of work of erection of steel girders. All the longitudinal steel girders of 45 m span have been erected but work of bracing has not been completed till date. The alignment of these girders is still to be done. Regarding 16 m span (PP-5\_PP-6), one longitudinal girder is still to be erected and bracing work is to be started. The agency pointed out that they are facing some problem in fixing the bracing and matter was discussed with Mr. Ganeshan of LTR and he was requested to give a solution of the problem being faced in fixing of bracings.

The progress of balance erection work was again reviewed with respect to the program discussed with Engineer-in-Chief, PWD two days back and it was felt that the activities are still slipping. As per that program, the entire erection work is to be completed by 28<sup>th</sup> March' 2016 and it was promised that the program will be further squeezed. It is regretted to point out that the work is not going on as per that program. The Project Manager of M/s. Simplex and senior Engineer of M/s. P&R were directed to enhance the various resources to expedite the progress of erection of work which is being delayed day by day.

Then, took the round of the entire site. First, the issue of fixing of street light poles was discussed at site on L.H.S. between CRPF red light and Najafgarh Drain and it was made very clear that at no location, pole's foundation should obstruct the service road or main carriageway. These poles should be either adjusted in the green belt or footpath as per site situation. The matter was also discussed with the EE (Elec.) and he was directed to immediately visit the site and sort out the problems being faced in installation of street light poles.

The SE, F-12 also discussed the issue of development of green belt/service road on R.H.S. where there is a huge encroachment. He was directed to prepare a detailed drawing showing therein the space available after construction of footpath and location of trees so that a suitable decision is taken. The matter was also discussed with Mr. Johri, Conservator of Forest on telephone and he was requested to issue the pending tree cutting permissions on priority due to which service road / Cycle Track may not be constructed at many locations.

Then, went to the A-1 side of elevated corridor, where on L.H.S. in certain portion, APP layer has been laid but no APP layer fixing was in progress at any location whether on L.H.S. or R.H.S. No activity of over deck cleaning was visible. It is regretted to point out that during various last inspections, it was committed that the entire elevated corridor, except nallah portion, will be completed by 15<sup>th</sup> March' 2016 but no efforts seems to be made in this direction by the agency. It is not understood, why the agency is delaying all these activities. The agency should have deployed more number of labour gangs at different locations for over deck cleaning so that enough front is available for fixing of APP layer. The SE, F-12 was directed to ensure that until and unless, proper over deck cleaning is done, which includes cutting of reinforcement and sealing with epoxy, removal of concrete splatters etc. the APP layer should not be allowed to be fixed. All these activities should be monitored span by span.

It was also directed, long back, to start the finishing of crash barriers/central verge surface by grinding of joints and filling of pin-holes but no such activity has been started. This is again a time consuming job and more gangs should be deployed at different locations to finish the surface of central verge/crash barrier. During last few inspections, the undersigned has been insisting for preparation of sample of central verge and crash barrier after painting but same has not been done. This is highly deplorable. It was again directed that a sample of 10 to 20 m

of central verge and crash barrier should be prepared after proper finishing and painting and SE F-12 should ensure the same.

In A-1 ramp and at few locations in central verge and crash barrier, proper line and level have not been achieved and it was directed to dismantle that much portion of central verge/crash barrier and recast the same.

Then, went to A-1 ramp, where on R.H.S. in certain portions, BC layer has been laid and on L.H.S. only DBM layer has been laid. The surface of BC layer was inspected and at few locations, it was felt that there seems to be some deficiency in proper compaction of the layer. The SE, F-12 was directed to immediately call the road agency for doing the needful. It has already been directed on number of occasion that while doing any bituminous work, one PWD/TPQA representative should be present at the hot-mix plant and other representative should be at site during laying of BC. This is very much required to ensure proper quality of work. Proper rolling should be ensured to have desired surface quality. SE F-12 should ensure the compliance of these directions.

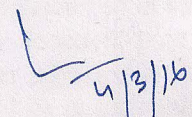
It was also again pointed out that while laying the BC layer, level wearing surface. across the expansion joint, should be checked using aluminium rod at different places so as to ensure that while fixing the expansion joint and filling the gap with concrete, perfect levelled surface is achieved to ensure jerk free movement over expansion joint. Moreover, in one carriageway, BC is laid in two passes and aluminium rod should be used, along the carriageway width, at the joint of two passes to ensure proper levelling of wearing course at the joint.

It was also directed that if at any place, APP Membrane is protruding above the BC surface that should be cut. It was also directed to fill the gap of BC layer and central verge/crash barrier to stop percolation of water which may damage the road surface later on. All these activities should be carried out from one end to the other end under close monitoring. A proper record of the same may also be kept to ensure strict compliance.

Where BC layer has been completed, kerb channel in the line of rain water spout should be made after fixing of rain water spout and making of khurras.

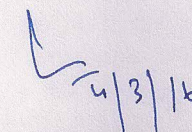
The status of load testing was also enquired which has been delayed by the agency very badly. The same should be started immediately.

It is felt that the agency is not trying to expedite the various activities for reasons best known to them and the SE, F-12 was directed to pursue the agency with a detailed program to expedite all the pending activities and monitor the same on daily basis.

  
d- Chief Project Manager  
(Flyover) 1, PWD

**Copy to:-**

- (1) The E-in-Chief (Projects), (Flyover) 1, PWD, 12<sup>th</sup> Floor, MSO Building, I.P. Estate, New Delhi for kind information please.
- (2) The Pr. Chief Engineer (Projects), PWD, 9<sup>th</sup> Floor, MSO Building, I.P. Estate, New Delhi for kind information please.
- (3) The OSD (PWD) to Minister, O/o of the Minister, Power, PWD, Health, Industries & Home, GNCTD, A-Wing, 7<sup>th</sup> Level, Delhi Secretariat, I.P. Extension, New Delhi-110002 for kind information please.
- (4) The Superintending Engineer, F-12, PWD for information & necessary action please.

  
o/c Chief Project Manager