

	<p align="center">CHIEF PROJECT MANAGER (FLYOVER) 1 PUBLIC WORKS DEPARTMENT GOVT. OF DELHI MUKARBA CHOWK, G. T. KARNAL ROAD, DELHI – 110033</p> <hr/> <p align="center">Ph: 011-27691871, Fax No.: 011-27691971 E-Mail: cpmpwddelhi1@gmail.com</p>	
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F. No. 23(28-I)/CPM/F 1/PWD/2016/535

Date: - 26-2-16

Inspection Note

Sub: - Inspection Note for the work of “Comprehensive Development of Corridor (Outer Ring Road) between Madhuban Chowk to Mukarba Chowk.”

Inspected the site of Madhuban Chowk to Mukarba Chowk on 25.02.2016 along with the SE, F-14 and Project Manager of M/s. ITD Cementation.

Started the inspection from L.H.S. near Prashant Vihar Market where rectification in footpath work was being done. The underdeck finishing was also in progress at few places. Following are the observations: -

Footpath Work:

1. While carrying out the rectification in footpath, it should be ensured that the geo-fabric is properly pasted in full width of the strip as it was observed that the glue has not been properly applied due to which sides of the geo-fabric strip is not properly pasted. This will not serve the required purpose.
2. The height of the footpath should be ensured as per UTTIPEC guidelines. Wherever, height of the footpath is more than required, that should be brought down unless and until there is any obstruction to that.
3. At few locations, the foundation of the electric poles have been placed inappropriately, which is obstructing the service road. This is not desirable. This matter has already been discussed with the Executive Engineer (Elec.) in the case of Vikaspuri to Meerabagh stretch and they are planning to install poles of more height at the end of ROW, in the area of ramps, so that it illuminate the whole carriageway and service road. If this is not possible then the foundation of the pole may be adjusted in the drain portion using the thickness of the drain wall also but in no case the pole foundation should not obstruct main carriageway or service road.
4. The 1.0/1.5 mtr. wide footpath to be constructed at the end of ROW, specially on LHS adjacent to A1 ramp, has not been started despite my continuous persuasion for last more than 6 months. This is a time consuming job and should be carried out in a gradual manner, as the residents might protest which has to be tackled. In front of the entry of each house, level of the paver blocks should match with the road levels.
5. More number of gangs need to be deployed for completion of footpath work as huge quantity is to be executed on L.H.S. and R.H.S.

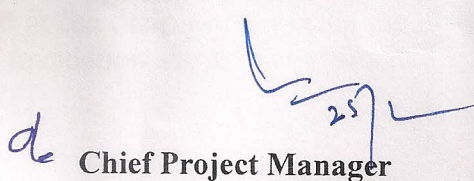
UNDERDECK FINISHING:

Reviewed the under deck finishing in detail and following are the observations: -

1. The grinding of the joints along lateral stitch should be done properly as at few places it has been left.
2. The joints of the horizontal and vertical fins of wings segments with the sides of the spine segments should be properly finished to have a fine and clean line. No extra filling should be allowed as the same may fall down due to vibration later on and also look very ugly. This point has been stressed upon by the undersigned on number of occasions earlier.
3. Thickness of any repair/putty should not be more as the same can fall later on.
4. The agency was directed to prepare one span after completing under deck finishing in all respects immediately and it was committed that spine P-2 to P-3 will be ready by Monday i.e. on 29.02.2016. The SE, F-14 and Engineers of the Ms/. ITD were directed to ensure that underdeck finishing is carried out span by span after getting checked from PWD officials before moving further.
5. Smaller holes along the lateral stitch should be filled properly as already directed earlier.
6. It was also directed that the deficiency list should be prepared which may require repair of fins, repair around spine's stubs where wing segments have rested, repair of shear stub protruding out from the diaphragm segments and should be attended Pier by Pier. This exercise is not going on in the desired manner. It was directed that these deficiencies should be attended in a systematic manner from one side to other and material to be used for repair should be properly checked by PWD as well as Engineers of the agency. It has always been advisable to monitor the repair works closely to avoid repetition of the exercises.
7. It was also directed to increase the number of gangs on under deck finishing.

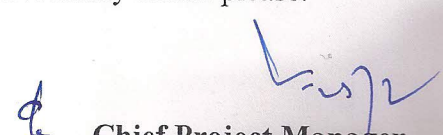
Other activities

1. The grit wash plaster was being carried out on brick work on the service road from K.N. Katju Marg to PWD site office. The line and level should be ensured. The grooves of the grit wash should also be repaired with the cement slurry simultaneously so that it is cured with the grit wash plaster.
2. The undersigned had directed to rectify the crash barrier casted between PWD site and Haryana Canal on L.H.S. as its line and level is very bad. The same is to be chiselled or dismantled and re-casted which is still to be repaired. The SE, F-14 was again directed to get it rectified immediately.


Chief Project Manager
(Flyover) 1, PWD

Copy to:-

- (1) The E-in-Chief (Projects), (Flyover) 1, PWD, 12th Floor, MSO Building, I.P. Estate, New Delhi for kind information please.
- (2) The Pr. Chief Engineer (Projects), PWD, 9th Floor, MSO Building, I.P. Estate, New Delhi for kind information please.
- (3) The OSD (PWD) to Minister, O/o of the Minister, Power, PWD, Health, Industries & Home, GNCTD, A-Wing, 7th Level, Delhi Secretariat, I.P. Extension, New Delhi-110002 for kind information please.
- (4) The Superintending Engineer, F-14, PWD for information & necessary action please.


Chief Project Manager