

	<p align="center"><b>CHIEF PROJECT MANAGER (FLYOVER) 1 PUBLIC WORKS DEPARTMENT GOVT. OF DELHI MUKARBA CHOWK, G. T. KARNAL ROAD, DELHI - 110033</b></p> <hr/> <p align="center"><b>Ph: 011-27691871, Fax No.: 011-27691971 E-Mail: <a href="mailto:cpmpwddelhif1@gmail.com">cpmpwddelhif1@gmail.com</a></b></p>	
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F. No. 23(24-I)/ CPM/F 1/PWD/2016/

525

Date: - 23-2-16

**Inspection Note**

**Sub: - Inspection Note for the work of "Comprehensive Development of Corridor (Outer Ring Road) between Mangolpuri to Madhuban Chowk".**

Inspected the site of Mangolpuri to Madhuban Chowk along with the PM, F-15 and Project Manager of M/s. ITD Cementation on 23.02.2016.

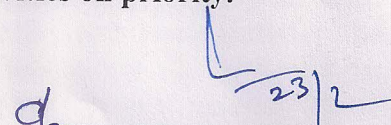
Started the inspection from A-2 side ramp up to A-1 side -2 ramp. Following are the observations:

1. As directed during earlier inspections, the deficiency list containing repairs of fins of diaphragm segments, repair in spine segments, where concrete has chipped off during post-tensioning, removal of plywood shutters, repair of shear stubs etc. should be prepared and attended on priority. It was intimated that the repair process has been started from A-1 pier side and completed upto P-30. However, during inspection, it was noticed that some repairs have still been left. It was directed that all these repairs should be monitored span by span and the labour should not be allowed to move to the next span till the earlier span has been completed in all respect.
2. As a part of under-deck finishing, it was pointed out that joints of the horizontal fins and vertical fins of wing segments with the sides of spine segments have to be properly finished. During longitudinal stitching, all these joints were sealed carelessly with some material like rendro-plug etc. which are now looking very odd/ugly. All this crude filling should be removed using grinder and joints finished properly after filling with proper material. These junctions should appear neat and clean and not like what is visible as on date. This is a time consuming activity and should be completed span by span under close supervision.
3. The cleaning of crash barriers has not been started. At lot of places, gunny bags etc., used to plug the gap during concreting, have to be removed. At number of places, the concrete slurry has spilled over the surface of fascia panel which should be cleaned. At few places, edges of the crash barrier or damaged grooves are to be repaired. This is again a time consuming activity and should be started immediately. In the crash barriers, where lifting hooks have been cut, the same should be covered with the epoxy material to avoid any rusting later on.
4. The repair around the stub portion where wing segments have rested, should be properly carried out as at few places, repair material has fallen down due to vibrations. It was the apprehension expressed earlier. At such locations, filling should be removed and repair redone properly to avoid its fall due to vibration in future.
5. At number of places, after completion of laying of bituminous layer (BC), kerb channel has been casted. It was advised to ensure that there is a proper slope in the kerb channel towards the bell mouth pipes.
6. The sample span of anti-carbonation paint of the colours approved has not been prepared which should be expedited.
7. It was also directed that while doing the under-deck finishing, proper grinding of lateral stitch should be ensured to remove any concrete slurry, finishing around stub, which have been repaired, etc. It was observed that at number of locations, the grinding has been left and surface was being finished. The PM, F-15 was directed to ensure that complete under-deck finishing is also monitored closely and supervised span by span so that one span is completed



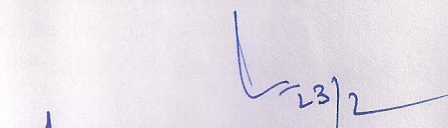
- in all respect. As already suggested, the smaller holes along lateral stitch should be filled up with some material/backer rod, as has been done in the stretch from Vikaspuri to Meerabagh, so that the same are not visible.
8. Regarding construction of footpath, it was again directed that all the laid footpath should be checked at regular intervals to ensure that the joints of the drain covers are properly sealed with mortar and geo-fabric fixed on top and sides so that bedding sand does not seep into the drain. This exercise is very important to be carried out to avoid settling of tiles at a later date. At few location on R.H.S., tiles appeared to be settled down. Entire stretch needs to be checked thoroughly.
  9. At number of places, tuck tiles are not continuous. One sample has been prepared by casting of tuck tiles pattern with mortar on the drain covers. It may not be very durable. It was also suggested that there can be other option of casting of tuck tiles pattern on drain cover with thermoplastic paint. The sample may be prepared accordingly as discussed at site. It may be ensured that colour of the tuck tile pattern over drain cover with concrete or thermoplastic paint is of yellow colour so that it merges with the tuck tile paver tiles.
  10. Wherever, tiles in footpath are being laid afresh, proper sealing of joints should be checked before allowing laying of tiles, as already directed on number of occasions. It should also be ensured that at the ends of footpath and near bus stops, its level is lowered down to merge with the road levels. The height of the footpath should also be ensured as per UTTIPEC guidelines.
  11. The issue of construction of toilets near pedestrian subway at Saraswati Vihar T-Junction was also discussed. The PM, F-15 was directed to submit an estimate of the toilet so that a decision is taken on the same.
  12. The issue of horticulture work was also discussed, as now horticulture work is to be executed under the supervision of PM, F-15. It was directed that the item of fixing of PVC net may not be required to be executed. The agency may be directed accordingly. The horticulture work should also be expedited.
  13. The PM, F-15 discussed the arrangement of fixing of rain water pipes below deck as two types of samples have been prepared. Matter will be finalised within a day or so after discussion with the Engineer-in-Chief, PWD and Pr. Chief Engineer (Projects).

**The agency was also directed to complete all the pending activities on priority.**

  
**Chief Project Manager**  
(Flyover) 1, PWD

**Copy to:-**

- (1) The E-in-Chief (Projects), PWD, 12<sup>th</sup> Floor, MSO Building, I.P. Estate, New Delhi for kind information please.
- (2) The Pr. Chief Engineer (Projects), PWD, 9<sup>th</sup> Floor, MSO Building, I.P. Estate, New Delhi for kind information please.
- (3) The OSD (PWD) to Minister, O/o of the Minister, Power, PWD, Health, Industries & Home, GNCTD, A-Wing, 7<sup>th</sup> Level, Delhi Secretariat, I.P. Extension, New Delhi-110002 for kind information please.
- (4) The Project Manager, F-15, PWD for information and n.a. please.

  
**Chief Project Manager**